



RABBIT LAKE TRAILHEAD IMPROVEMENTS

SEAWOLF ENGINEERING

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 Technical Leads: Matt Westbrook and Kelly Yanoshek



Abstract

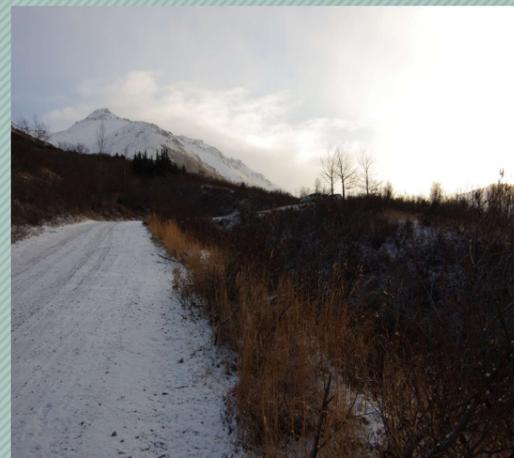
With the current narrow road and limited parking facilities to access Rabbit Lake trailhead, increased stress has caused wear and tear on Canyon Road. The roadway is not suitable for the current influx of hikers nor will meet the needs of the projected incoming hikers. The implementation of the Rabbit Lake trailhead improvements will increase safety, coordination and satisfaction for the future of Chugach State Park users.

History

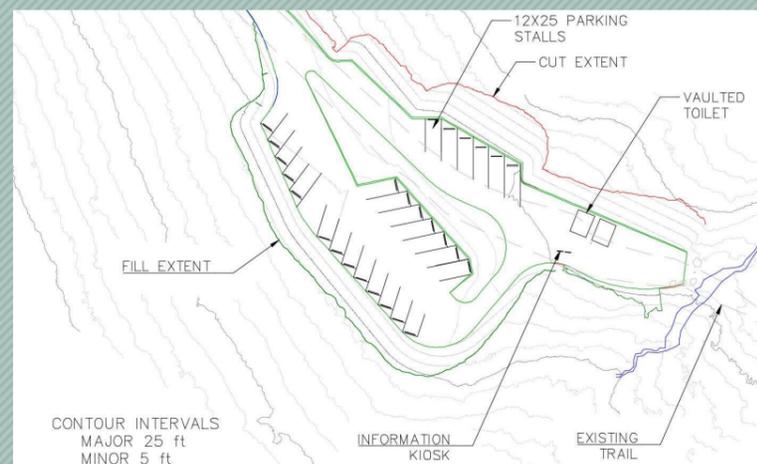
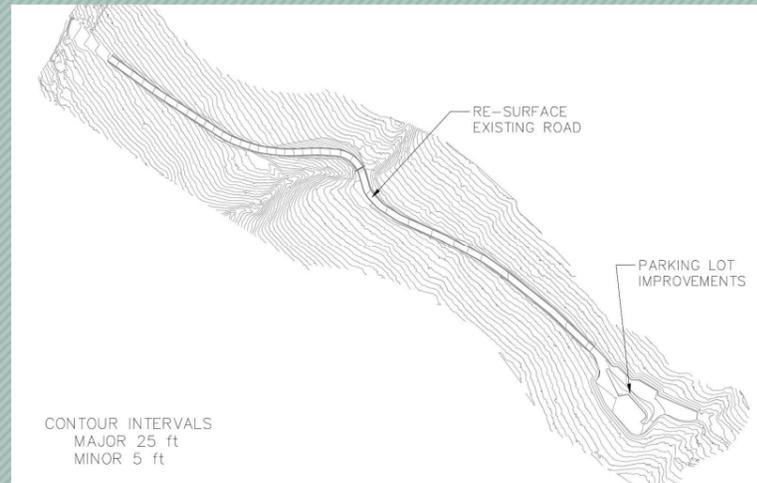
- 1970 Governor Keith Miller signed the Alaska Session law to establish the Chugach State Park an official state park.
- 1980 There was a legal dispute among private land owners making it illegal to access the Rabbit Lake trailhead.
- 1986 Chugach State Parks Trail Plan suggested 15 to 20 parking spaces and toilet facilities.
- 2002 Small parking space is created.
- 2006 The Great Land Trust created a campaign to increase trail access.
- 2007 Rabbit Lake Trail became legalized.
- 2009 Trail Management Policy was completed by the Division of Parks and Recreation.

Project Description

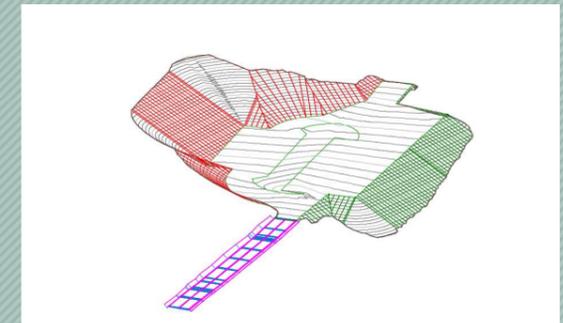
The trailhead improvements will include parking lot infrastructure, toilet facilities, barrier rocks and information kiosks. Up to 1,400 linear feet of Canyon Road will be widened to 20 ft. and recycled asphalt will be used to pave the road. The Canyon Road connection to the trailhead needs restoration to decrease dust, rutting and erosion. Recycled asphalt material will be the most economical and beneficial material to create a safer access road to enter the park land. Drainage ditches with rock lining will be placed on the sides of the road to mitigate drainage on to Canyon Road. Cut and fill from widening Canyon Road will be re-used to maintain vegetation along the roadway. This reused vegetation will provide added erosion and drainage control and give the trail a more natural appearance. The design of trails requires a balance between keeping nature intact and durability. The Rabbit Lake trail improvements will keep with the natural flow of the mountain side while providing stability on the roadway for improved safety.



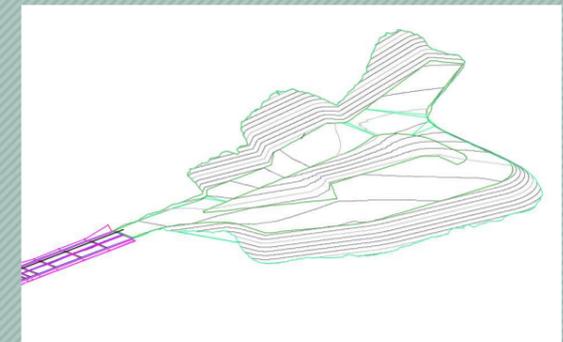
Parking Location



Design Alternatives



Alternative 1



Alternative 2

COST ESTIMATE

| | |
|-----------------------------|--------------|
| BASIC BID | \$333,550.00 |
| PROJECT CONTINGENCY | \$33,400.00 |
| DESIGN SERVICES | \$50,100.00 |
| CONSTRUCTION ADMINISTRATION | \$50,100.00 |
| PROJECT TOTAL | \$467,150.00 |



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